



Shipper Built Unit Guidelines Agreement

The purpose of this document is to standardize procedures and expectations for shipper built ULDs. Pre-built units tendered as a shipment to UA Cargo or accepting agent must be built by qualified staff, and the team must ensure that the units are “Ready for Carriage.”

United reserves the right to amend these guidelines based on updates to the UA Cargo Manual and IATA Guidelines.

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ULD Compatibility Chart

IATA Code	ULD Type	Aircraft Type				Max Gross LB	Max Gross KG
		777	787	767-300	767-400		
LD2	DPE	✗	✗	✓	✓	2,400 LB	1,088 KG
LD3	AKE	✓	✓	✗	✗	3,500 LB	1,588 KG
LD3	RKN	✓	✓	✓	✓	3,500 LB	1,588 KG
LD4	DQP	✓	✓	✓	✓	5,400 LB	2,450 KG
LD8	DQF	✓	✓	✓	✓	5,400 LB	2,450 KG
LD11	PLA	✓	✓	✗	✗	7,000 LB	3,175 KG
LD11	ALP	✓	✓	✗	✗	7,000 LB	3,175 KG
LD9	RAP	✓	✓	✓	✓	10,200 LB	4,626 KG
LD4	FNA	✓	✗	✗	✓	5,400 LB	2,450 KG
LD7	PAG/PAJ	✓	✓	✓	✓	10,200 LB	4,626 KG
P6	PMC	✓	✓	✓	✓	11,100 LB	5,034 KG

Restrictions

Do not use any tracking devices within the ULD without prior written authorization.

AKEs marked with a green “not approved for 787” sticker are not permitted on 787 aircraft.

Effective May 31, 2024, FQF Pallets are retired from the United Cargo ULD fleet.

The following cannot be shipped as Shipper Built Units

- Live Animals.
- Human Remains.
- Firearms, swords and other weapons.
- HAZMAT/Dangerous Goods; with the exception of items listed in the IATA Dangerous Goods Regulations 9.1.4

Containers

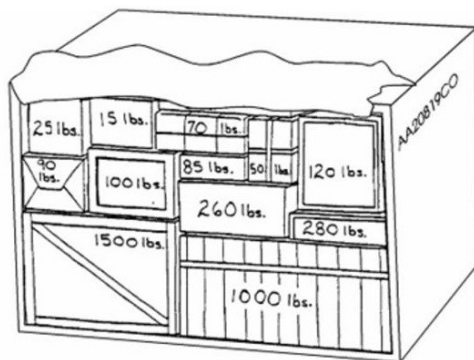
Cleanliness

It is mandatory to remove any previous tags, stickers, labels, tape, or unreadable placards in effort to reduce misrouting. Additional measures should be taken to ensure ULDs are clean and do not contain any loose items, such as skids, nets, debris, or straps that may damage the cargo.

Loading Containers

When loading ULDs the maximum gross weight and floor bearing weight must always be observed to guarantee the ULD's capacity to contain the cargo safely and securely. For proper load distribution, careful pre-planning must be ensured to prevent damage and shifting.

Heavy items should be placed evenly on the floor of the ULD to prevent shifting.



- To provide maximum cubic utilization and prevent loads from shifting during loading ULDs should be loaded along the back and side walls first, then toward the center.
- To limit the potential of tipping in transit heavy pieces should be distributed evenly throughout a ULD.
- To prevent tears in the composite material sharp items should not be placed against side walls.
- To prevent tipping, heavy items are not allowed in the wing/wedge/dog leg of ULDs with angled walls (DPE, AKE, RKN, DQF)
- Using any method or device to widen the door opening, allowing pieces larger than the door to fit, is prohibited. This activity may cause damage to the container frame and render the ULD unfit for service.
- When maneuvering a ULD onto or off a truck using a forklift, ensure that the forklift blades are fully inserted beneath the container before lifting. This measure prevents damage to the ULD and mitigates the risk of tipping or falling.

Securing Cargo in Containers

To prevent cargo shifting and damage to personnel or equipment all cargo must be properly secured. ULDs must be secured with a United Cargo strap, or similar TSO certification, rated to 5000 lb. / 2267 kg. United Airlines cargo straps are orange in color and have "United Airlines Cargo" written on them in bold black typeface.

When securing top heavy or unstable cargo the cargo must be secured using straps, and if possible, with other cargo. ULDs have tie-down fixtures available for tying down cargo.



Wrapping Containers

United Cargo does not require ULDs to be wrapped. However, if ULDs are wrapped, the following guidelines must be followed, disobeying any of the guidelines may result in refusal.

- Wrapping must be at a minimum of 3 inches above the base and should not cover any part of the base.
- Full container ID code markings: ODLN & TSO (Manufacturer's) plate must be legible.
- Wrapping material must not contain adhesive that leaves residue or markings on the ULD.
- Stickers, tape, or other markings, beyond carrier attached branding, are not permitted.
- Concealing damages with wrapping material is not permitted. Only apply wrapping material to containers that are fully serviceable.

Storage

Containers that are not in use must maintain closed, strapped, and latched doors at all times. Containers shall be stored on designated dollies, storage racks, and wooden pallets to mitigate risk of damage. Under no circumstance should containers be positioned on the ground when located in outdoor settings.

Stacking

It is imperative to stack containers in a matter that mitigates risks to personnel, property, or equipment from damage. Containers may only be stacked exclusively with those of the same type. Mismatching of containers is not permitted.

Serviceability of Containers

Aluminum and composite ULD container serviceability check:

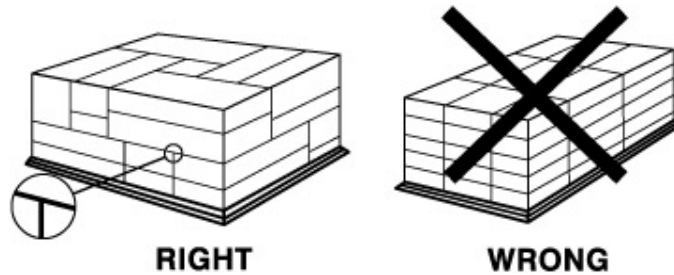
- Side Panels: Max 1 hole or tear per side, max 3"; 10" distance between each damage on adjacent panels.
- Roof / Base: No holes, rips, or tears on roof or base panel.
- Panels Rivets / Fasteners: Max 2 missing per row per panel.
- Base Rivets / Fasteners: Max 1 missing rivet on each row/side. Max 4 missing rivets. No missing corners.
- Frame / Extrusions: Max 1 hole max of 3/4". No warping or deflections of the frame.
- Door / Canvas: Max 3 holes, rips, tears not exceeding 6", minimum 10" between each damage.
- Velcro Door Straps: Free of any cuts, fraying at seams, holes, or other damage with prevents strap grip.
- TSO Plate: Legible manufacturers TSO plate / sticker in place.

Pallets

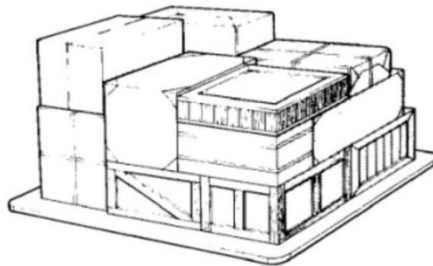
Build-Up

Pallets should be built on a pallet dolly, roller-equipped loading platform (ball mats), or on a side entry moveable roller deck or build up pallet, a pallet pit, or on the ground. For pallets built on the ground care must be taken to protect the pallet from damage. The pallet should be put on a clean, flat surface and additional care must be exhausted to ensure the pallet and contents are not damaged. Pallets should never be pushed or dragged when transported.

Use a bricklayer method to stabilize the cargo. Interlocking layers of similar sized pieces secure the load and avoid shifting. (See illustration below.)



Build light pieces on top, with the heavier items around smaller pieces to maintain equal distribution and mitigate damage. Items with wheels must be strapped and when possible chocked or elevated on blocks or skids. (See illustration below)



It is imperative to maintain a two-inch clearance around the perimeter of the pallet at all times to ensure proper attachment of the net locking assembly.

Height Verification

The maximum height for any pallet or container carried on United Cargo aircraft is 64 inches. This measurement includes the height of the pallet board, net, and straps. **DO NOT BUILD ANY PALLET THAT EXCEEDS 64 INCHES IN HEIGHT. ANY PALLET EXCEEDING 64 INCHES WILL BE REJECTED.** It is recommended that a measuring tool be used to verify that the whole top surface, including all sides, do not exceed 64 in / 160cm in height.

Serviceability of Pallets

A properly built pallet consists of a serviceable pallet and serviceable cargo net. The cargo net is the primary cargo restraint while the cargo straps are used as a supplemental restraint. Pallets must always be shipped with a net and the serviceability of the pallet and net assembly shall always be verified.



ULD pallet serviceability check:

- Base / Edge Rails: No holes, cracks, punctures, missing corners, or warping of the pallet greater than 1”.
- Rivets / Fasteners: Max 1 missing rivet per row/ side; max 4 total
- Tie Down Points: No less than 3 undamaged tie-down points on each side of the stud location.
- TSO Plate: Legible manufacturers TSO data in place.

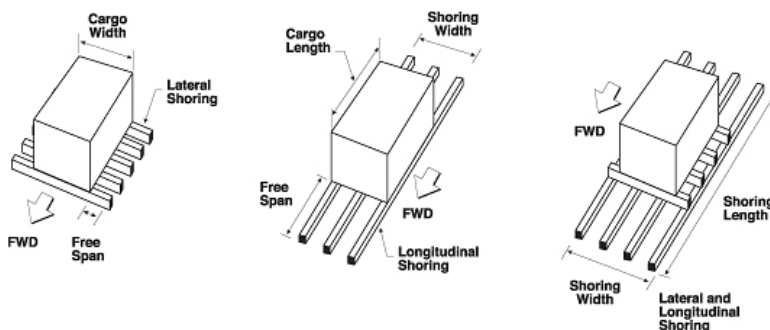
Nets / straps / accessory serviceability check:

- Nets: Free of any breaks or cuts. All lock clips are present and functional. The expiration is not exceeding or is not older than 3 years based on the date of the manufacture.
- Cargo Straps: Free of any cuts or fraying at the seams. Free of broken or damaged hardware. United straps within 5 years of the date of manufacture; non-United straps within three years of the date of manufacture.

Shoring, Spreader Boards, Skids

Shoring (load spreading) can become necessary for heavy (typically over 1,000 kg. / 2200 lb.) concentrated loads in order to meet either the applicable aircraft area load or linear/running load limitations, or both. More information on shoring, spreader boards, or load spreading can be found in the IATA ULDR Operating Specification 6/09.1

Spreader boards (1" x 12" timber, wooden pallets) can be placed under cargo to increase the surface contact area and spread the pressure per square foot over a greater surface area. This practice will also prevent the bowing of the pallet when straps and nets are tightened to lock the unit in the aircraft. (See examples below.)



The most effective shoring method consists of placing a wooden pallet onto the aircraft pallet prior to laying the cargo. Double-faced wooden pallets are recommended to ensure proper weight distribution.

Securing Cargo on Pallets

To prevent cargo shifting and damage to personnel or equipment all cargo must be properly secured. Use additional tie-downs to secure cargo when pallets are overbuilt or have unstable items with a minimum of two (2) Cargo straps (one in each direction) to ensure movement on the pallet is restricted. (Straps should be rated to 5000 lb. / 2267 kg). For small packages, use corner pieces before strapping. For irregular shapes, consider an accessory pallet rack. To prevent expansion or slipping, tightly strap the top layers. For items that may be crushed, insert a flat intermediate floor, and ensure the pallet's outer tracks are clear. Alternatively, pre-assemble small packages onto wooden pallets to form stackable units, fitting the aircraft's main deck contour.

The cargo net is the primary cargo restraint while the cargo straps are used as a supplemental restraint. Pallets must always be shipped with a net and the serviceability of the pallet and net assembly shall always be verified.

Cargo Protection

Cargo protection is applied to prevent weather damage to cargo caused by rain, snow, etc. Depending on the cargo, United Cargo authorized single sheet and double sheet protection:

Pallet single sheet protection is to be used when:

- There is no cargo identified as presenting the possibility of liquid leakage; and,
- Cargo is not loaded directly onto the pallet's surface.

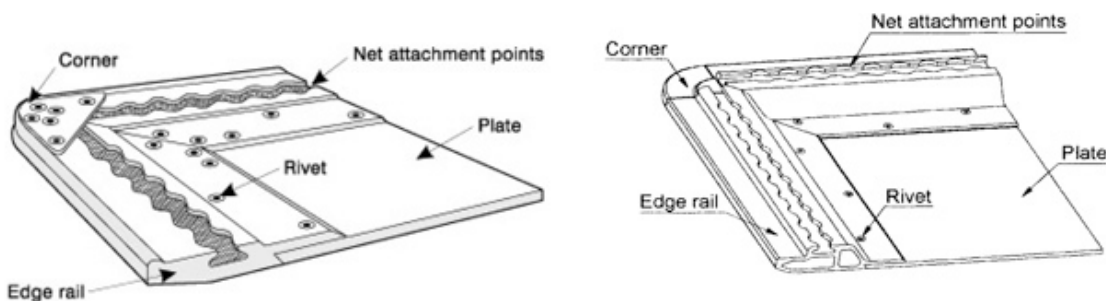
Pallet double sheet protection (bottom up and top down) is to be used when either:

- There is cargo identified as presenting the possibility of liquid leakage: cans, drums, bottles or similar, or fish and seafood items, etc. classified as WET cargo; or,
- There are fiberboard (carton) boxes loaded directly onto the pallet's surface.

Exception: Special attention must be given to perishable shipments often packed in specialized boxes which allow for ventilation of contents. Ventilation must be maintained throughout the duration of such shipments and therefore these shipments should not be wrapped with plastic sheets.

Pallet Net Attachment Points

Cargo built up on a pallet shall not block the use of any part of the net attachment point, TSO information, bar codes, or any other regulatory markings on the pallet. Additionally, it should not obstruct the area of the pallet edge rail outside the net attachment point (tie down point), which is used within the aircraft to secure the pallet to the aircraft floor locks.



Pallet Care and Storage

All pallets must be inspected prior to use. When not in use, separate pallets by type and store them in racks to avoid damage. Any unserviceable pallet must be immediately returned immediately to United Cargo.

Equipment Return

ULD containers and pallets should be returned in a clean state, free from debris, with nets securely bagged or affixed to the equipment. Straps are to be neatly rolled, and if pallets are stacked upon return, they must be securely strapped together.

Specific Freight Types

Tall Loads

Tall loads are defined as a load that is twice as tall as its narrowest horizontal dimension i.e., 60 inches high and 25 wide at its narrowest point would be considered a tall load and could have a center of gravity that makes it susceptible to tip/move, especially on takeoff and landing.

Such tall cargo should be palletized fully enclosed/blocked on potential tipping sides by other cargo that is higher than the center of gravity of the tall piece. As a rule, at a minimum this support cargo's height should be 50% above the narrowest width of the tall piece and the top of the tall cargo.

If supporting of cargo is not possible, the tall cargo must be shrink-wrapped, banded and palletized to a wooden or similar pallet with a wider base than the cargo, to protect against the chance of tipping. It can also be strapped to the pallet itself using cargo straps protecting against movement.



Cargo on Wooden or Plastic Pallets

If multiple pieces are tendered/loaded on a wooden or plastic pallet they must be shrink wrapped and securely banded to a skid.

The contents should be "banded" to the skid so that everything feels like one solid unit, without any shifting.

Use edge protectors or scrap cardboard to protect the shipment.



Barrels and Drums

Drums and barrels without a flat base that have a lip on the bottom (of all sizes) must be shrink wrapped and securely banded to a skid to avoid any chance of exceeding the structural limitations of the aircraft floor and reduce the risk of puncture when being removed.

Drums with a flat base that are loaded adjacent to the edge of a pallet are at risk of puncture from aircraft restraint hardware. These must be banded secured to a wooden or similar pallet to raise them 4 inches from the pallet base.



Seafood

All seafood shipments contained within ULDs must adhere with all refrigerant and packaging rules. ULDs must be lined with a polyethylene or plastic liner with absorbent material between the liner and the seafood unit. Shipments in ULDs must be loaded straight with the correct end up. Shipments in ULDs must be able to withstand the weight of any stacking without crumpling, crushing or leaking.

Gel packs, dry ice, and wet ice are the only acceptable refrigerants. Shippers who wish to utilize wet ice as refrigerant must be pre-validated and comply with specific inner packaging, outer packaging, and ULD packaging requirements as outlined on the Perishable Foodstuff Acceptance Checklist. (See unitedcargo.com).

- Seafood must be tendered in durable watertight packaging and packaged to withstand up to 48 hours intra-U.S. transit time and up to 72 hours international transit time.
- Seafood shipping units must be able to withstand stacking to a height of 56 inches (142 cm) without crumpling.
- Units must have wax-impregnated or waterproof coating inside or outside.
- Units must have leak-proof construction with gusseted corners at the top and bottom. Top must extend fully over bottom.
- A minimum of one 4-mil (or two 2-mil) plastic or polyethylene liner(s) must line the inside of each shipping unit.
- Absorbent material must be placed between the liner and the inner bag.
- Seafood (except live seafood) must be completely sealed in one 4-mil (or two 2-mil) bag(s). The inner bag seal must be leak-proof even when tipped or inverted.
- Fish with sharp claws, fins or projections must be packaged with a corrugated liner so claws, fins or projections do not come into contact with the inner bag.
- Minimum of two bands must be wrapped around the width of each unit.
- Styrofoam boxes must be protected by a sturdy fiberboard over pack container.
- Canisters/buckets must be leak-proof even when tipped or inverted.

Temp Control ULDs

All Temp Control ULD (RAP and RKN) must be built to current IATA Temp Control Regulations and follow the ODLN (Operational Damage Limit Notification) on the unit for Serviceability.

All Temp Control BUP that are built up on/in a non-temperature controlled ULD must follow this document for serviceability and build requirements and the IATA Temp Control Regulations.

Additional Guidelines

Straps

To apply tie down equipment to restrain a complete load before applying the cargo net, the following criteria can be used.

Cargo must have sufficient contact area for straps passing over, through or around the piece of cargo and straps cannot come in contact with sharp or cutting edges.

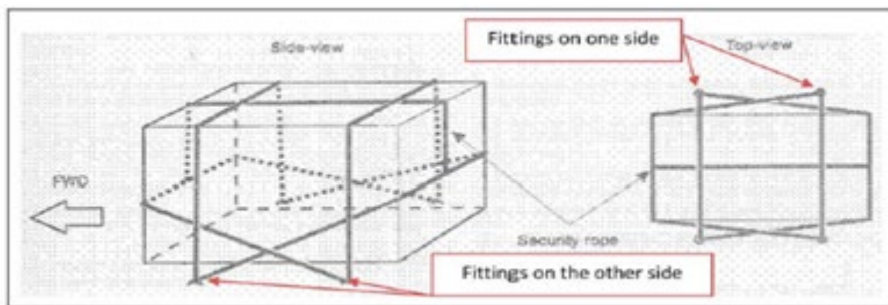
When applying tie-down on the pallet, ensure they don't bend the pallet's edge rail upward.

4-5 straps (2 on each side minimum) can be used to bind the load together. Where possible use edge protectors to protect and extend the support offered by the straps. Straps should be a minimum of 20 inches from the edge of the pallet and placed equidistantly apart.

For more complex loads, care must be taken to provide protection and avoid tie-down straps bearing in a horizontal direction of restraint, so they will not slide down accidentally. A security rope should be used if the load itself does not hold the straps in place (see below). This rope should be tied securely to the angled strap as shown to stop the strap from slipping down the pallet. A lashing rope or rope with a minimum thickness of ½ inch is recommended.

Items loaded onto a pallet must be secured in a manner that prevents the freight from moving or shifting.

Final inspection shall verify all straps are sufficiently and approximately equally tensioned. A practical way to assess it is pulling each strap at 90 degrees from its direction of restraint:



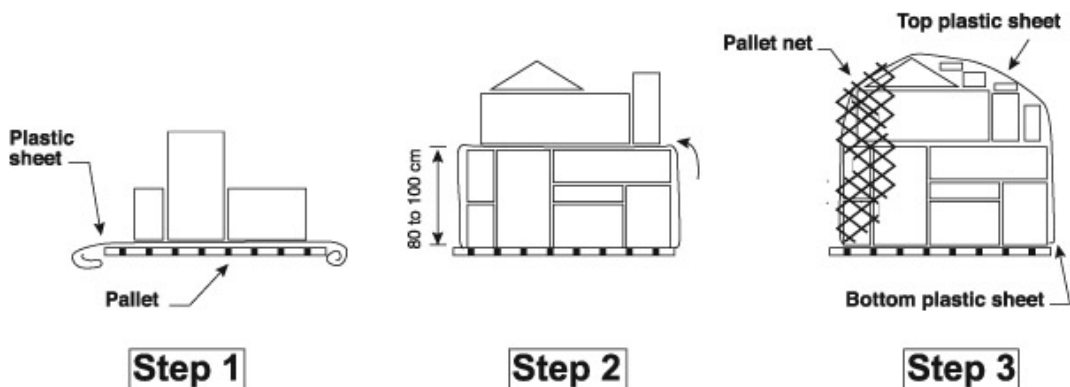
*****As a reminder nets act as the primary restraint! Straps act as a secondary restraint and must be under the net*****

Plastic Wrap

Clear or see through wrapping is preferred to allow United to inspect the load and build at acceptance. The process for applying plastic wrap to cargo on pallets is as follows:

- 1) Lay one plastic sheet (larger than the pallet) on the pallet before loading cargo.
- 2) Load cargo onto the pallet carefully to avoid any holes or punctures.
- 3) After pallet build-up, raise the bottom sheet edges all around the pallet, attaching them with adhesive tape or by the weight of the upper tier's cargo.
- 4) Lay down a separate top sheet (as shown below) and then install the net. Ensure top sheet effectively overlaps the bottom sheet to ensure watertight.

Exception: Special attention must be given to perishable shipments often packed in specialized boxes which allow for ventilation of contents. Ventilation must be maintained throughout the duration of such shipments and therefore these shipments should not be wrapped with plastic sheets.



Shrink Wrap for Stability of Freight

The plastic stretch wrap should first be applied around the base of the pallet load (bottom three inches must be left clear to avoid interfering with pallet locks) and continued upward around the load. Overlap each by 50% and make sure it's tight to prevent shifting, get assistance especially when starting to wrap the pallet to ensure an effective wrap of the pallet.

Clear or see through wrapping is preferred to allow United to inspect the load and build at acceptance.

- To start, tuck the beginning under a piece of freight to keep it in place, at least three inches from the base of the pallet.
- Start wrapping and slowly working up the pallet overlapping each rotation by 50%
- Pull the roll tightly before going around each corner.
- Twisting the wrap as you go around will increase the strength of it. Maintain overlapping if you use this twisting process.
- Ensure the wrapping is tight to ensure secure packaging.
- Tuck in or secure end piece of plastic.



Construction of Sandwich Pallets

Sandwich pallets can be used if their construction conforms to the principles below and only if the freight loaded on the pallet can be secured to prevent any shifting and movement during transport or flight.

- 1) Place base pallet onto build-up system or build up pallet.
- 2) Place four wooden skids onto the base pallet evenly spaced. (This may be repeated up to a maximum height of three wooden skids.)
- 3) Position a single top pallet directly in line above the base pallet.
 - Using more than one aircraft pallet on top, offset, to extend the length of the top pallet is not allowed. There is no reliable method to ensure offset pallets remain stable and do not shift in transport.
- 4) Attach two straps evenly spaced to the base pallet, stretch the straps over the length of the top pallet and attach to a similar location on the base pallet directly opposite. Attach three more straps evenly spaced to the base pallet, stretch these over the width of the top pallet and attach to a similar location on the base pallet directly opposite. Tighten all straps sufficiently to prevent any movement.
- 5) Once all straps are tightened, ensure that the platform is stable, and the top pallet does not move or shift before loading any cargo on the sandwich pallet.

Overhang

Overhang is defined as any cargo that exceeds the length or width of the pallet. Overhangs may be utilized to optimize cargo capacity; however, aircraft cargo pits configurations vary, and overhanging cargo may not fit on certain aircraft. All cargo must fit securely inside of the net attached to the pallet, including any overhang. **Pallets with overhang that do not fit all aircraft may experience transit delays due to weight and balance requirements.**

As a general guideline, freight must be raised one inch for each inch of overhang on each or either end. For example, a shipment that is 147 in (374cm) long exceeds the pallet length by 22 in (56cm) or 11 in (28cm) on each end. This piece must be raised at least 11 in (28cm) to conform to the contour of the aircraft.

Aircraft	Overhang Allowed
767	No overhang allowed
777	17 in (43cm) either side
778	17 in (43cm) either side

Demurrage and Accessorial Charges

See www.unitedcargo.com for current charges.

Compliance with United Cargo Shipper Built Requirements

United Cargo reserves the right to check all listed points above before accepting the goods for carriage.

United Cargo reserves the right to revoke the authorization to accept shipper built ULDs with immediate effect at any time.

Current versions of this document will be stored on Unitedcargo.com under 'Container Specifications.'

Signoff portion is on page 15.

United Cargo BUP Waiver and Release from Liability

1. NOTICE TO CUSTOMER

You, the customer of United Airlines, Inc. and United Cargo (“United”), or your shipper, agent, consolidator, freight forwarder, logistics provider, or similar, with respect to shipper built cargo ULDs, may only tender a cargo shipment for transport by air on United that is presented as a fully built-up Unit Load Device (“ULD”), which is fully secured and professionally built-up to recognized air cargo industry standards and best practices, and which built-up ULD, at a minimum, conforms to IATA ULD Regulation Section 9.8.4 and United Cargo’s Shipper Built ULD Guidelines published on Unitedcargo.com (together, the “Regulations”).

2. TENDER OF A FULLY BUILT-UP ULD

If you fail to tender to United a professionally built-up ULD, United shall have the following recourse, which by your signature below you indicate your irrevocable agreement:

- A- United has the right to reject a built-up ULD that fails to meet the Regulations or is otherwise not fully secured and built to recognized air cargo industry standards. Your shipper, agent, consolidator, freight forwarder, logistics provider, or similar must collect such ULD and return it to United’s facility for shipment once they have fully re-built the ULD in conformance with the Regulations and to recognized air cargo industry standards.
- B- United may re-build a ULD that fails to meet the Regulations or is otherwise not fully secured and built to recognized air cargo industry standards. If you choose this option, you hereby agree to pay to United a ULD re-build fee, which is currently \$125 per ULD and is subject to change at any time with or without notice.
- C- If neither option is selected or if United chooses to board on a United flight a ULD that fails to meet the Regulation or is otherwise not fully secured and built to recognized air cargo industry standards, and there is any subsequent claim for cargo loss, United hereby disclaims any liability under the airway bill or other shipping agreement, for any such claim for cargo loss, and you understand that you will be solely liable for any such claim. You further hereby agree to waive any claim, and fully release and hold harmless United for any claim, for cargo loss, if United transports a ULD tendered for shipment by you or your shipper, agent, consolidator, freight forwarder, logistics provider, or similar, that fails to meet the Regulations or is otherwise not fully secured and built to recognized air cargo industry standards.

3. HAZARDOUS MATERIALS. DANGEROUS GOODS AND OTHER LIMITATIONS

Loading of hazardous materials or dangerous goods on any shipper built ULD is strictly prohibited, unless listed below

Restrictions on contents in Shipper Built ULDs

- No HAZMAT/Dangerous Goods with the exception of:
Items listed in the IATA Dangerous Goods Regulations 9.1.4

The following cannot be shipped as Shipper Built ULDs

- Live Animals.
- Human Remains.
- Firearms, swords and other weapons.

4. LIABILITY

You and your shipper, agent, consolidator, freight forwarder, logistics provider, or similar hereby assume liability for all cargo loss or damage, or claims for the same, as well as for any fines or fees levied by any regulatory body or agency, arising from or in connection with: (i) the build-up of any shipper built ULD, (ii) the lack of “Ready for

Carriage” status, or (iii) with respect to hazardous materials or dangerous goods, any shipments that do not correspond with their accompanying documentation or that fail to include or display the required accurate documentation. You and your shipper, agent, consolidator, freight forwarder, logistics provider, or similar do hereby waive and release United from all claims for cargo loss or damage, including those claims made by third parties.

5. PIECE COUNTS FOR A SHIPPER BUILT ULD

Due to regulatory requirements, it may be necessary to identify the number of pieces loaded into a ULD, pursuant to the shipper’s load and count (SLAC) set forth in the air waybill, including both the nature and quantity of goods as well as number of pieces. In such case this identification will not constitute any confirmation that United has received such number of pieces included in, or identified as the contents of, any shipper built ULD.

United reserves the right to revoke your authorization at any time, with immediate effect, to tender shipper built ULDs for air transport on United flights.

By signing below, you and your shipper, agent, consolidator, freight forwarder, logistics provider, or similar agree to adhere to all guidelines contained in this Waiver and Release from Liability and any subsequent amendments, as well as all United, FAA, TSA, ICAO and IATA regulations regarding shipper-built ULDs.

**This Waiver and Release covers the locations set forth below.
Please check the relevant box and fill in the details of ALL covered locations using the UA IATA, Customer Branch Code below:**

- Worldwide** _____
- Country(s)** _____
- Individual location(s)** _____

United Representative
Signature: _____

Shipper/Agent/Freight Forwarder –
Signature: _____

Printed Name: _____

Printed Name: _____

Title: _____

Company and Title: _____

Date: _____

Date: _____
